

Standard Symbol

BMP Objectives

- Soil Stabilization
- Sediment Control
- Tracking Control
- Wind Erosion Control
- Non-Storm Water Management
- Materials and Waste Management

Definition and Purpose

A temporary stream crossing is a structure placed across a waterway that allows vehicles to cross the waterway during construction, minimizing, reducing, or managing erosion and downstream sedimentation caused by the vehicles.

Appropriate Applications

Temporary stream crossings are installed at sites:

- Where appropriate permits have been secured (1601 Agreements, 404 Permits, and 401 Certification).
- Where construction equipment or vehicles need to frequently cross a waterway.
- When alternate access routes impose significant constraints.
- When crossing perennial streams or waterways causes significant erosion.
- Where construction activities will not last longer than one year.

Limitations

- Will usually disturb the waterway during installation and removal.
- May require Regional Water Quality Control Board (RWQCB) 401 Certification, U.S. Army Corps of Engineers 404 permit and approval by California Department of Fish and Game. If numerical-based water quality standards are mentioned in any of these and other related permits, testing and sampling may be required. If monitoring related to these numerical-based water quality standards is not addressed in the contract documents, contact the Resident Engineer (RE).
- Installation may require dewatering or temporary diversion of the stream. See BMP NS-2, "Dewatering Operations" and NS-5, "Clear Water Diversion."
- May become a constriction in the waterway, which can obstruct flood flow and cause flow backups or washouts. If improperly designed, flow backups can increase the pollutant load through washouts and scouring.

- Use of natural or other gravel in the stream for construction of Cellular Confinement System (CCS) (refer to figure at the end of the section) ford crossing will be contingent upon approval by fisheries agencies.
- Ford crossings may degrade water quality due to contact with vehicles and equipment.
- CCS should not be used in excessively high or fast flows.
- Upon completion of construction activities, CCS blocks must be removed from stream.

Standards and Specifications

General Considerations

Location of the temporary stream crossing shall address:

- Site selection where erosion potential is low.
- Areas where the side slopes from highway runoff will not spill into the side slopes of the crossing.

The following types of temporary stream crossings shall be considered:

- Culverts - Used on perennial and intermittent streams.
- Fords - Appropriate during the dry season in arid areas. Used on dry washes and ephemeral streams, and low flow perennial streams. CCS, a type of ford crossing is also appropriate for use in streams.
- Bridges - Appropriate for streams with high flow velocities, steep gradients and/or where temporary restrictions in the channel are not allowed.

Design and installation requires knowledge of stream flows and soil strength. Designs shall be prepared under direction of, and approved by, a registered civil and/or structural engineer. Both hydraulic and construction loading requirements shall be considered with the following:

- Comply with the requirements for culvert and bridge crossings, as contained in the Caltrans Highway Design Manual, particularly if the temporary stream crossing will remain through the rainy season.
- Provide stability in the crossing and adjacent areas to withstand the design flow. The design flow and safety factor shall be selected based on careful evaluation of the risks due to over topping, flow backups, or washout.
- Avoid oil or other potentially hazardous waste materials for surface treatment.

Construction Considerations:

- Stabilize construction roadways, adjacent work area and stream bottom against erosion.

- Construct during dry periods to minimize stream disturbance and reduce costs.
- Construct at or near the natural elevation of the stream bed to prevent potential flooding upstream of the crossing.
- Install temporary sediment control BMPs in accordance with sediment control BMPs presented in Section 4 to minimize erosion of embankment into flow lines.
- Vehicles and equipment shall not be driven, operated, fueled, cleaned, maintained, or stored in the wet or dry portions of a water body where wetland vegetation, riparian vegetation, or aquatic organisms may be destroyed, except as authorized by the RE, as necessary to complete the work.
- Temporary water body crossings and encroachments shall be constructed to minimize scour. Cobbles used for temporary water body crossings or encroachments shall be clean, rounded river cobble.
- The exterior of vehicles and equipment that will encroach on the water body within the project shall be maintained free of grease, oil, fuel, and residues.
- Disturbance or removal of vegetation shall not exceed the minimum necessary to complete operations. Precautions shall be taken to avoid damage to vegetation by people or equipment. Disturbed vegetation shall be replaced with the appropriate soil stabilization measures.
- Riparian vegetation, when removed pursuant to the provisions of the work, shall be cut off no lower than ground level to promote rapid re-growth. Access roads and work areas built over riparian vegetation shall be covered by a sufficient layer of clean river run cobble to prevent damage to the underlying soil and root structure. The cobble shall be removed upon completion of project activities.
- Any temporary artificial obstruction placed within flowing water shall only be built from material, such as clean gravel, that will cause little or no siltation.
- Drip pans shall be placed under all vehicles and equipment placed on docks, barges, or other structures over water bodies when the vehicle or equipment is planned to be idle for more than one hour.
- Conceptual temporary stream crossings are shown in figures at the end of this section.

Specific Considerations:

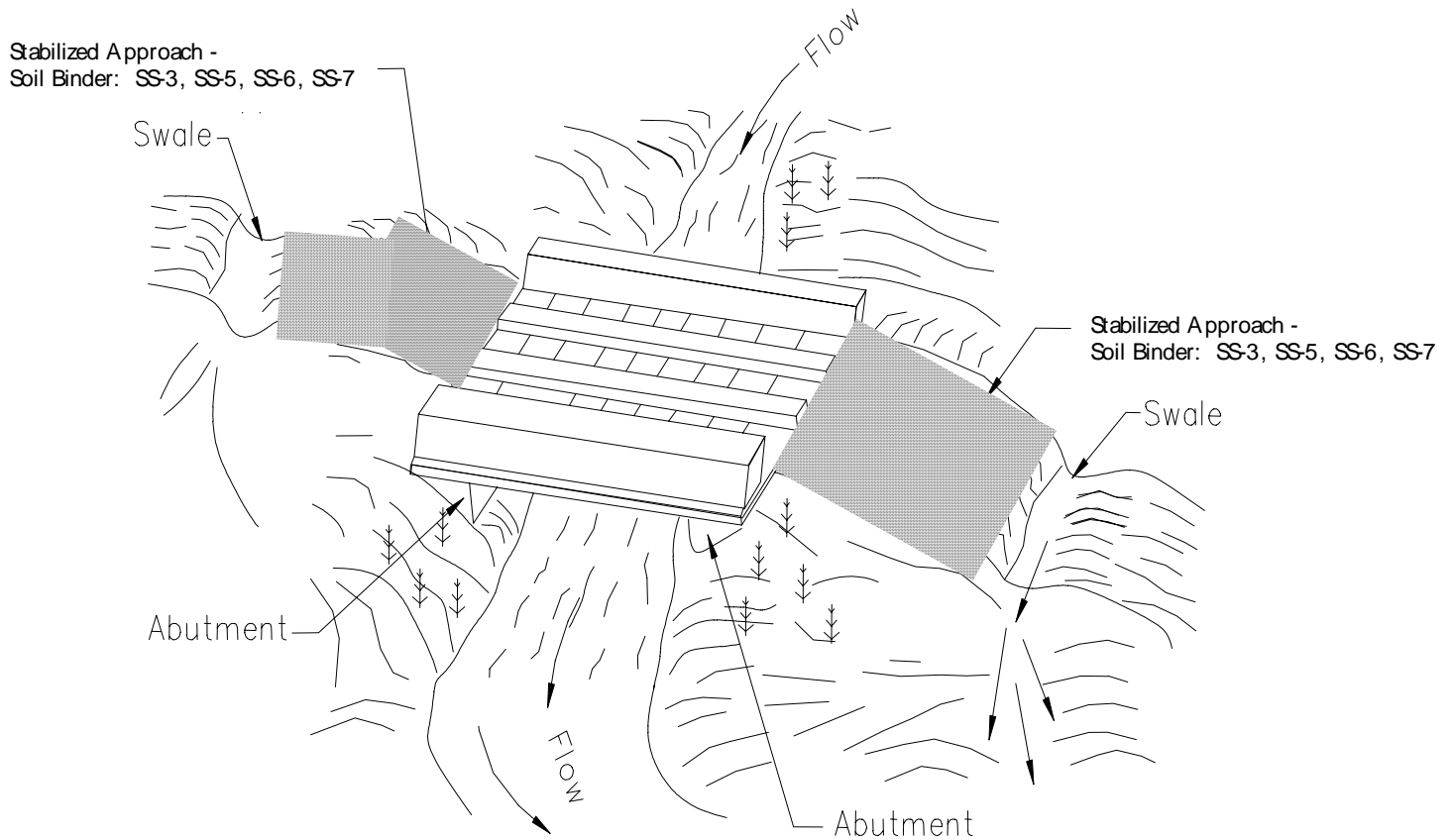
- Culverts are relatively easy to construct and able to support heavy equipment loads.
- Fords are the least expensive of the crossings, with maximum load limits.
- Temporary fords are not appropriate if construction will continue through the rainy season, if thunderstorms are likely, or if the stream is perennial.

- CCS crossing structures consist of clean, washed gravel and cellular confinement system blocks. CCS are appropriate for streams that would benefit from an influx of gravel; for example, salmonid streams, streams or rivers below reservoirs, and urban, channelized streams. Many urban stream systems are gravel-deprived due to human influences, such as dams, gravel mines, and concrete channels.
- CCS allow designers to use either angular or naturally-occurring, rounded gravel, because the cells provide the necessary structure and stability. In fact, natural gravel is optimal for this technique, because of the habitat improvement it will provide after removal of the CCS.
- A gravel depth of 152 to 305 mm (6 to 12 inches) for a CCS structure is sufficient to support most construction equipment.
- An advantage of a CCS crossing structure is that relatively little rock or gravel is needed, because the CCS provides the stability.
- Bridges are generally more expensive to design and construct, but provides the least disturbance of the stream bed and constriction of the waterway flows.

Maintenance and Inspection

Maintenance provisions shall include:

- Periodic removal of debris behind fords, in culverts, and under bridges.
- Replacement of lost protective aggregate from inlets and outlets of culverts.
- Removal of temporary crossing promptly when it is no longer needed.
- Inspection shall, at a minimum, occur weekly and after each significant rainfall, and include:
 - Checking for blockage in the channel, debris buildup in culverts or behind fords, and under bridges.
 - Checking for erosion of abutments, channel scour, riprap displacement, or piping in the soil.
 - Checking for structural weakening of the temporary crossing, such as cracks, and undermining of foundations and abutments.



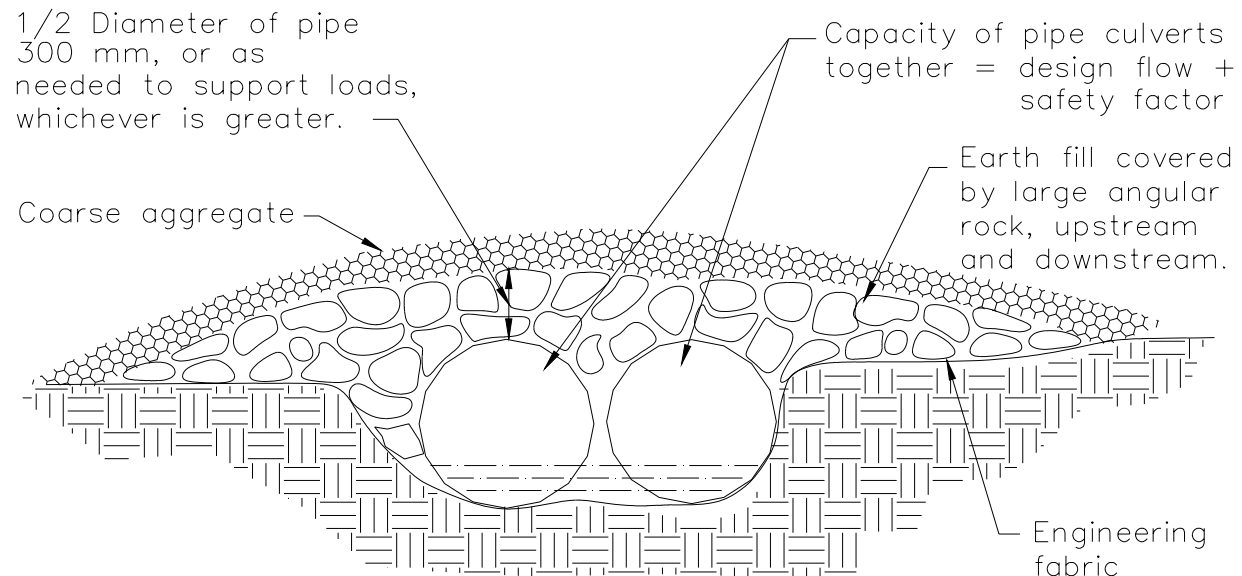
NOTE:

Surface flow of road diverted by swale and/or dike.

TYPICAL BRIDGE CROSSING
NOT TO SCALE

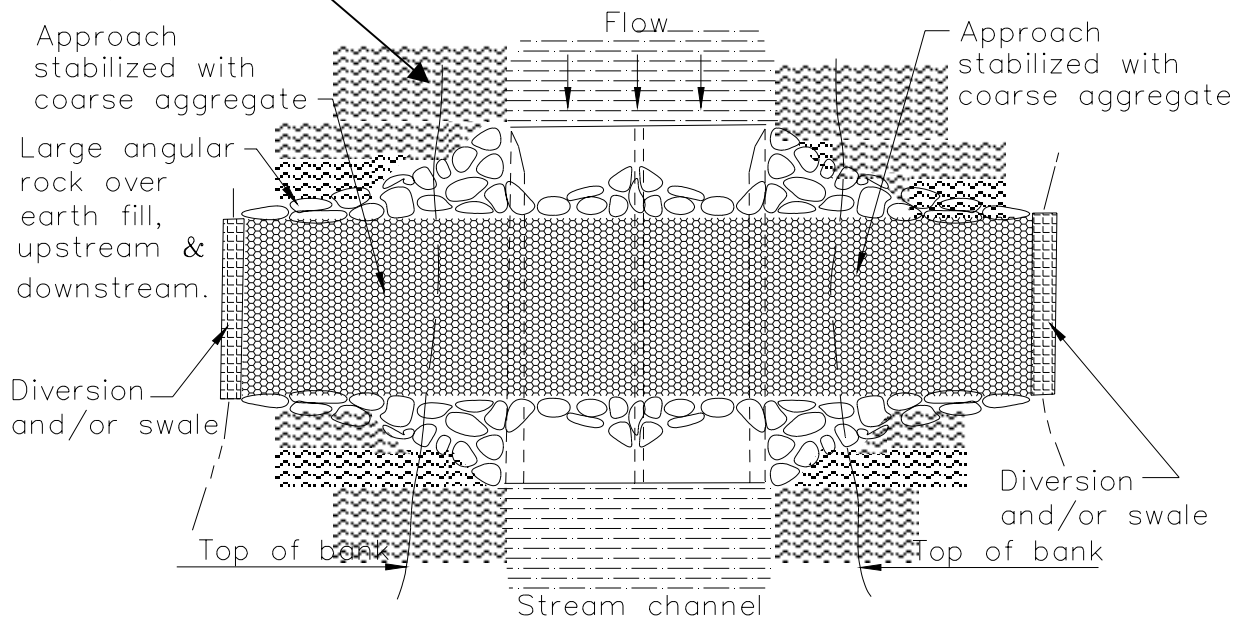
Temporary Stream Crossing

NS-4



Soil Binder: SS-3, SS-5, SS-6, SS-7

ELEVATION



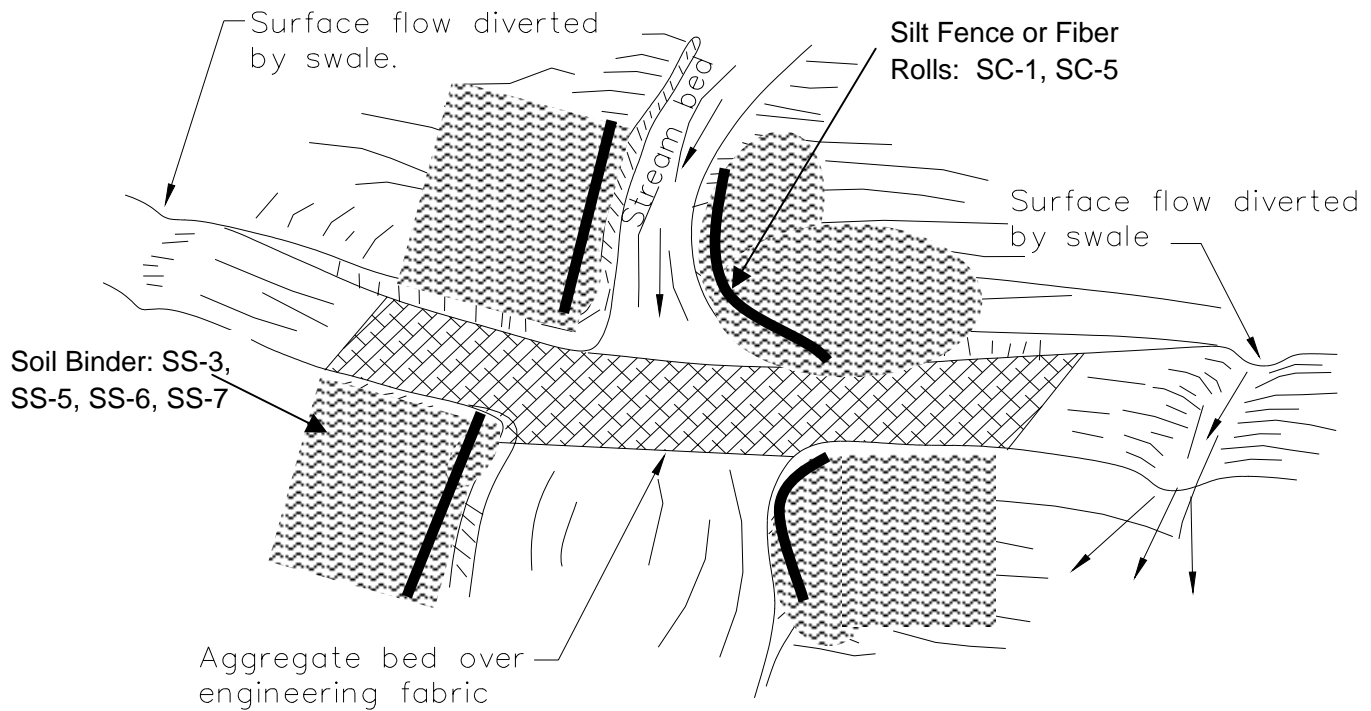
PLAN VIEW

TYPICAL CULVERT CROSSING NOT TO SCALE

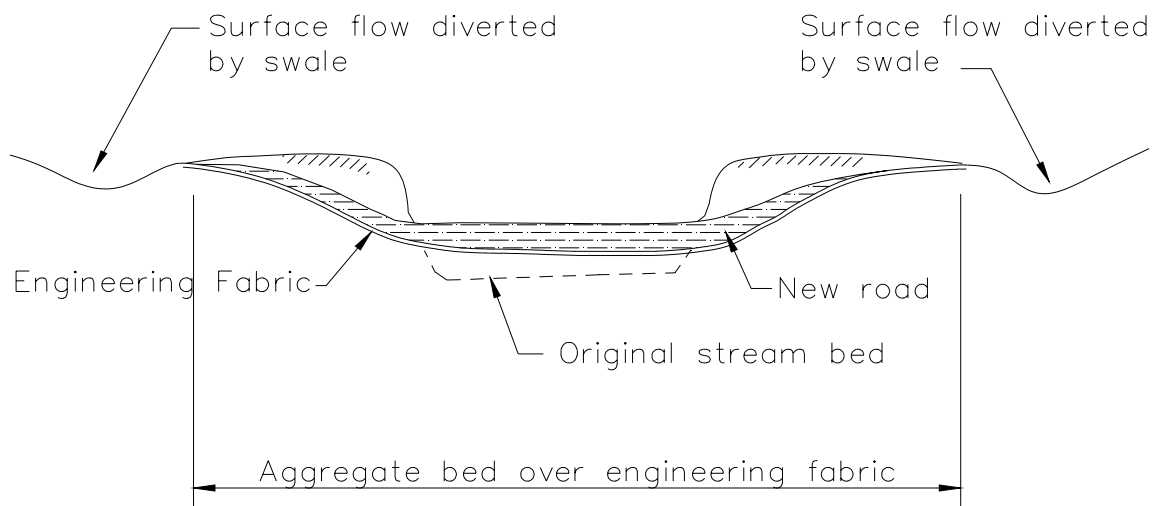


Temporary Stream Crossing

NS-4



Aggregate approach
1:5 (V:H) Maximum slope on road



TYPICAL FORD CROSSING
NOT TO SCALE



CELLULAR CONFINEMENT SYSTEM